

From: Stephanie Rock  
Sent: Monday 24 October 2022 22:28  
To: Liam  
Subject: Observation to an Bord Pleanála

<b>AN BORD PLEANÁLA</b>	
LDG- <u>058 512-22</u>	
ABP- _____	
25 OCT 2022	
Fee: € <u>50</u>	Type: <u>cash</u>
Time: <u>15.36</u>	By: <u>hand</u>

I wish to make an observation regarding the proposed substation Glendale Confey Leixlip Ref NC29N. 306587

My concerns relate to the following issues:

1. The site is a residential area and has been maintained as such for over forty years. There are grounds over Cope Bridge, non residential, which would be more suitable and cause less disruption.
2. The area is used as a play area by local children and is also the location of many species of flora and fauna as well as many mature trees.
3. The size of the substation building. Why is it so much larger than other substations? Are other uses planned for this building?
4. The traffic situation in the area is chaotic at present. There is a proposal to open Cope Bridge to two way traffic, where is the infrastructure to cope with this extra load?
5. There are two cycleways proposed over Cope Bridge, it is unclear where the cycle lanes will begin or end?
6. Where will construction machinery be housed during the process. Also where will construction workers park during the process?

I reserve the right to add to this submission if it appears to impact further on living conditions.

Regards,  
Stephanie Rock  
28 Glendale Estate  
Leixlip  
W23P834

7. In what condition will our green area be left after completion and who will maintain same?

8. What are the hours of work during construction and what will the noise levels be?

*Stephanie Rock*  
*24/10/22*

E Mail: [REDACTED]

## Observation details

### 5. Grounds

Please describe the grounds of your observation (planning reasons and arguments). You can type or write them in the space below. There is no word limit as the box expands to fit what you write.

You can also insert photographs or images in this box. (See part 6 – Supporting materials for more information).

The residents of Glendale met and discussed observations of the Strategic Infrastructure Development application by CIE Draft Order DART + West Electrified Heavy Order 2022. From this meeting a committee was formed with the above mentioned individuals and the observations are summarised as follows:

#### 1. The Cycle Lane / Pathway

As Referenced in Work No.28.4 Drawing No.WP028 7m depth of current Green Space at the front of Glendale will be lost and replaced with a new wall, embankment and hedging, and a cycle lane / pathway (see Figure 1.2). The residents have observed that this will have a significant impact on the existing green space that is currently used for recreational activities as well as impacting on current fauna and flora. It will particularly impact on the mature trees that residents have grown and maintained over many years (see Figure 1.1).

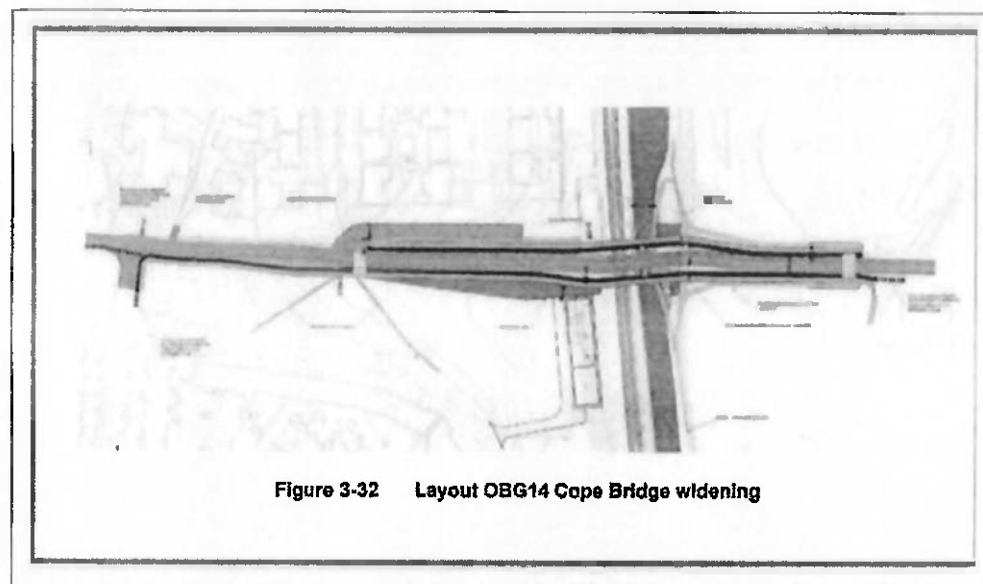
## 5. Grounds

Figure 1.1



The residents have observed that there could be a possibility of keeping the existing boundary and hedgerow and add the cycle lane / pathway running along the existing green space. The residents are not clear if this has been considered.

Figure 1.2



## 5. Grounds

The residents have observed that there is no clear plan for further development of the Cycle Lane / Pathway as it ends abruptly either end of the proposed infrastructure.

The residents have also observed a similar cycle lane / pathway on the opposite side of the road / bridge and question the need for a cycle lane / pathway on both sides of the road. Using one side only, the current train Station side would have least impact on all parties. The residents question if this has been considered.

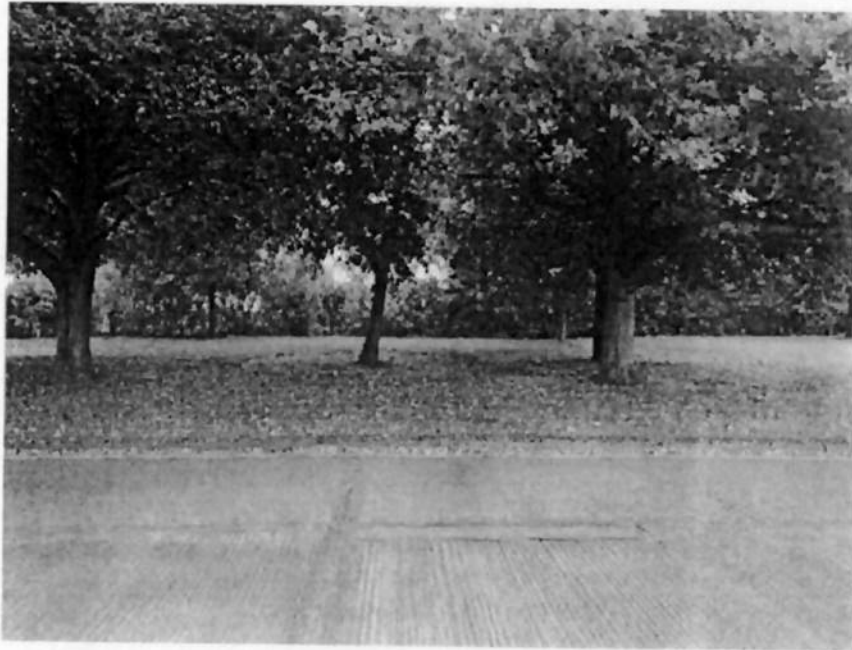
### **2. The New Boundary / Embankment**

The application makes reference to a new wall and embankment

Work No.28.4 Drawing No.WP028 This new wall and embankment will replace mature hedgerows and trees. The Residents observations are that it is not clear at what level of maturity will plants and trees be planted and trees can take many years to grow and provide natural scenery. Currently the mature boundary screens off heavy traffic from view (See Figure 2.1).

Figure 2.1

## 5. Grounds



### **3. The Compound**

The application makes reference (**Work No. 28.6 Drawing No. WP028**) to a compound for storage of machinery and equipment being placed on the front green space for a significant amount of time (see Figure 3.1). This will hugely impact on the current green space used by residents for recreation such as children playing sports, residents walking their dogs and people having picnics. This green space has been maintained for many years by the residents both with residents' money and volunteered time with a particular focus on biodiversity (see Figure 3.2). This compound will also impact negatively on the natural fauna, flora and mature trees. The residents have observed the magnitude of this compound and believe it will also have a negative impact on residents in terms of noise pollution; create an eyesore to what has been a well established, picturesque green area; create additional traffic and cause congestion in the estate (see Figure 3.3); create additional traffic of



## 5. Grounds

HGVs causing a danger to children crossing the roads; and affect current access to train station in a safe manner.

Figure 3.1

Temporary material storage area for this bridge has been provided as follows:

- OBG14.



Figure 5-311 Plan of proposed temporary material storage area at OBG14

Figure 5-312 3D view of proposed temporary material storage area at OBG14  
OBG 14 deck reconstruction's main compound (CC-STR-08-7480) is located in Ladbroke Grove at Ch 74+000. Vehicles delivering to the site will use the R14B road to connect to the R14B located south of the compound, as shown in Figure 5-314.

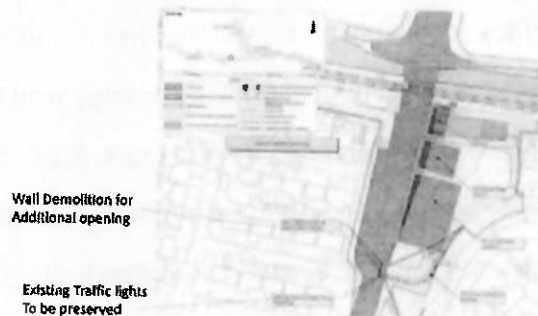


Figure 5-313 OBG14 structure compound

## 5. Grounds

Figure 3.2



Figure 3.3



## **5. Grounds**

The residents have observed recent strategic infrastructure developments

such as development of underground pipes, using the field opposite the train station on the far side of the canal as a compound for storage. This has had very little impact on residents once consulted with private land owner and the residents question whether this option has been explored and feel it would be a preferred first choice for everyone's health and well being.

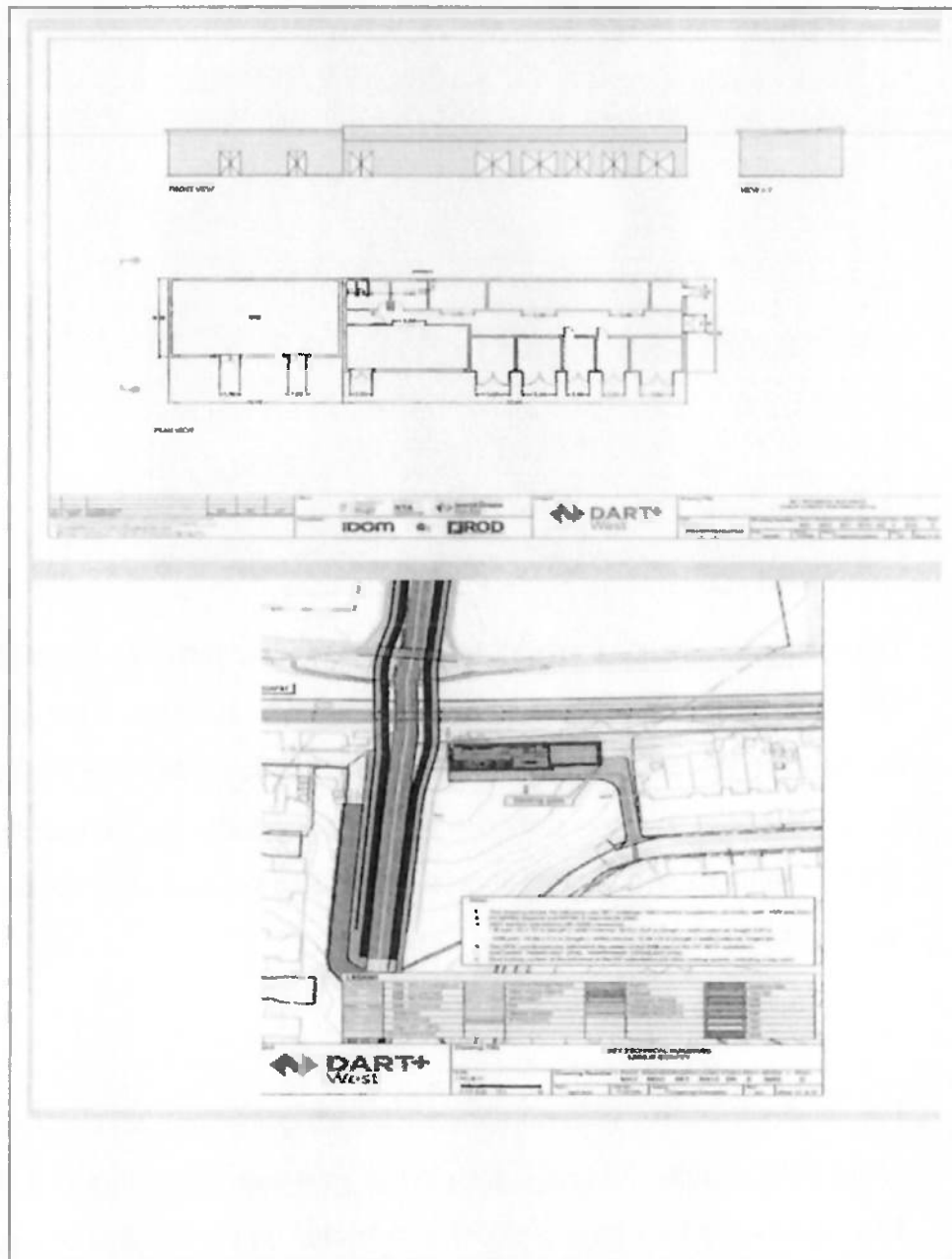
### **4. The Substation**

The residents have observed in the application on section **Work No. 28.5 Drawing No. WP028** the proposal to build a permanent substation on the front green area in Glendale (see Figure 4.1). This will greatly impact residents due to the significant loss of green area that is currently used for recreational purposes. This will also impact the mature hedgerows and trees. This green area has been maintained by residents for many years, both financially and through volunteering of time. Residents had been in discussion about increasing the biodiversity of these lands and these have had to be put on hold with this proposal imminent.



## 5. Grounds

Figure 4.1



## 5. Grounds

Figure 4.2



The residents have observed the height, width and depth of this building which they believe will be a significant eyesore to the current natural landscape. The residents are aware of land that is owned by CIE further along the tracks and they question if another location was considered. The residents have not been able to observe or get clarity on the function of the substation or its necessity.

The residents have observed an access road going from an internal estate road, across the green area to the substation (See Figure 4.1). This access road will have a significant impact on loss of green space currently used for recreational purposes by residents. This internal estate road already experiences severe congestion and can prove very difficult for residents to exit the estate. This is in part due to people parking in the estate and then travelling by train (see Figure 4.3). Adding

## 5. Grounds

additional traffic to this area will be an extreme hardship on residents and also add additional issues with road safety in particular for the young residents of the estate.

Figure 4.3

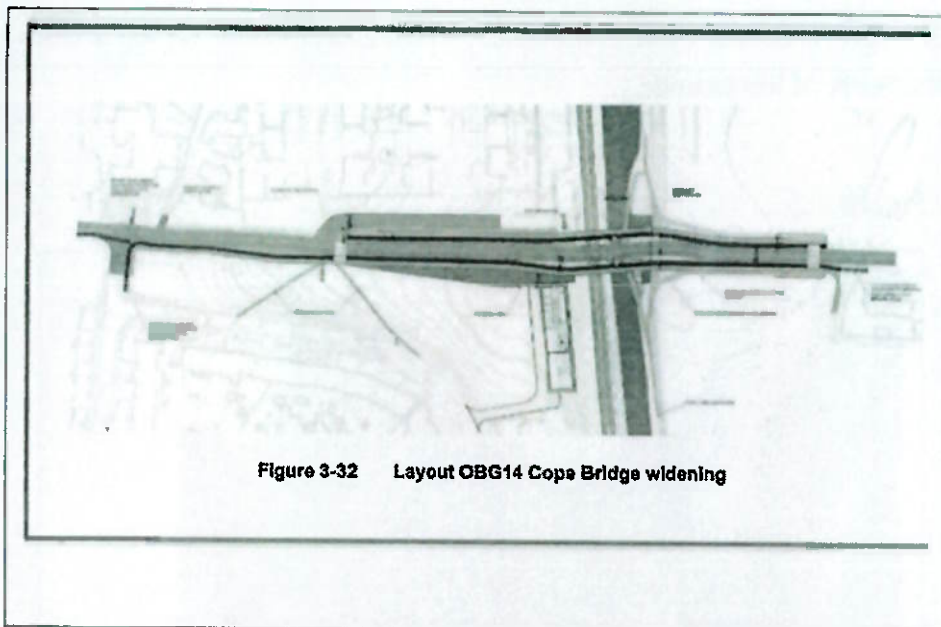


### **5. Pedestrian Crossing:**

The Residents have not been able to observe clear pedestrian crossing planned at the entrance to Glendale Estate across the main road to the many public amenities such as the Library and Youth centre. They have observed Traffic lights at either end of the bridge on the plans but it is not clear if pedestrian (see Figure 5.1). However, many people cross at the entrance of Glendale Estate and with the increase of traffic once it is a two-way system this will make it much more dangerous to cross.

## 5. Grounds

Figure 5.1



### 6. Parking:

Currently Glendale Estate experiences a high volume of cars parking on the roads in order to access the train station (see Figure 6.1). The current Confey Train Station has a small car park which is rarely full to capacity due to costs of parking. With the planned improvements to the train service the Residents have not been able to observe a clear plan to manage parking for train users.

## 5. Grounds

Figure 6.1



### **7. Personal Affect:**

The Residents have observed in the application significant timeframes set out for completions of works. Each footbridge will take an estimated 46 weeks (see figure 7.1). Some of these planned works are also scheduled for at night time. This will create a huge disturbance to residents especially those near the train tracks. The detour of traffic away from Confey Bridge while it is closed will have a significant impact to Residents travel time as the detour will lead Residents to already congested areas such as Leixlip Village, Lucan Village and Kelly's Town Lane (see Figure 7.2). There is not a clear plan in place on how to manage traffic. It will also add a large cost to journeys with people having to use the M50 toll bridge, where normally they would not have had to access.

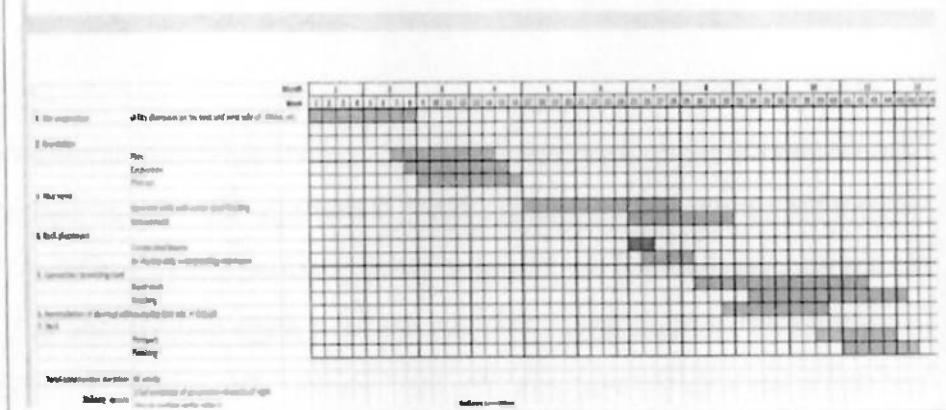


## 5. Grounds

Figure 7.1

As shown in the following design chart, the total construction duration is estimated at approximately 46 weeks per footbridge.

Most works will be performed during the project construction working hours as set out in Section 5.2.1. Part of the foundation work will be performed during full railway weekend possessions. Placement of the steel deck, in-situ top slab, and placement of parapets will be performed during 4 weeks of night possessions and 2 full weekends possessions on the railway. Further details of working hours for each construction phase have been provided in the table below. As mentioned previously, the construction work of the new East and West footbridges could be carried out before the arch reconstruction of the existing road bridge OBG14.





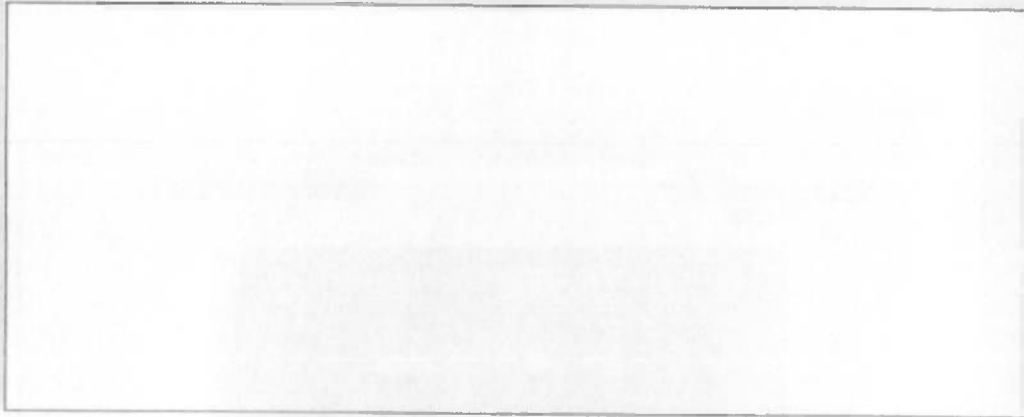
## 5. Grounds

Figure 7.2



While the Residents of Glendale want to support improvements to Infrastructure, there are significant observations made by them that would warrant re-consideration of current plans and future meetings to be held with all parties involved. The Residents of Glendale would be happy to attend such meetings.

## 5. Grounds



## Supporting materials

6.	<p>If you wish, you can include supporting materials with your observation. Supporting materials include:</p> <ul style="list-style-type: none"><li>• photographs,</li><li>• plans,</li><li>• surveys,</li><li>• drawings,</li><li>• digital videos or DVDs,</li><li>• technical guidance, or</li></ul> <p>other supporting materials.</p> <p>You can insert photographs and similar items in your observation details: grounds (part 5 of this form).</p> <p>If your supporting materials are physical objects, you must send them together with your observation by post or deliver it in person to our office. You cannot use the online uploader facility.</p>
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## Fee

7. You **must** make sure that the correct fee is included with your observation.

**Observers (except prescribed bodies)**

strategic infrastructure observation is €50.

there is no fee for an oral hearing request

## Oral hearing request

8. If you wish to request the Board to hold an oral hearing, please tick the "Yes, I wish to request an oral hearing" box below.

You can find information on how to make this request on our website or by contacting us.

If you do not wish to request an oral hearing, please tick the "No, I do not wish to request an oral hearing" box.

Yes, I wish to request an oral hearing

☐

No, I do not wish to request an oral hearing

☐

## Final steps before you send us your observations

9. If you are sending us your observation using **the online uploader facility**, remember to save this document as a Microsoft word or PDF and title it with:

the case number and your name, or

the name and location of the development and your name.

This also applies to prescribed bodies sending an observation by email.

If you are sending your observation to us by post or delivering in person, remember to print off all the pages of this document and send it to us.

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<b>FEM – Received</b>		<b>SIDS – Processed</b>	
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Date		Date	

**Notes**